

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 23rd April 2019

Report of
Assistant Director, Planning,
Highways & Transportation

Contact Officer:
Andy Higham
David Gittens
James Clark

Ward:
Highlands

Ref: 18/01539/FUL

Category: Full Application

LOCATION: 70A and 72 The Ridgeway, Enfield, EN2 8JB

PROPOSAL: Demolition of existing buildings and redevelopment to provide a total of 9 self-contained flats (6 x 2-bed, 3 x 3-bed) within a 2-storey building including accommodation in roof space, creation of a new vehicular access and associated car parking and landscaping on site.

Applicant Name & Address:

Mr Richard Collins
Landvest Developments Limited
Fusion Studio
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Agent Name & Address:

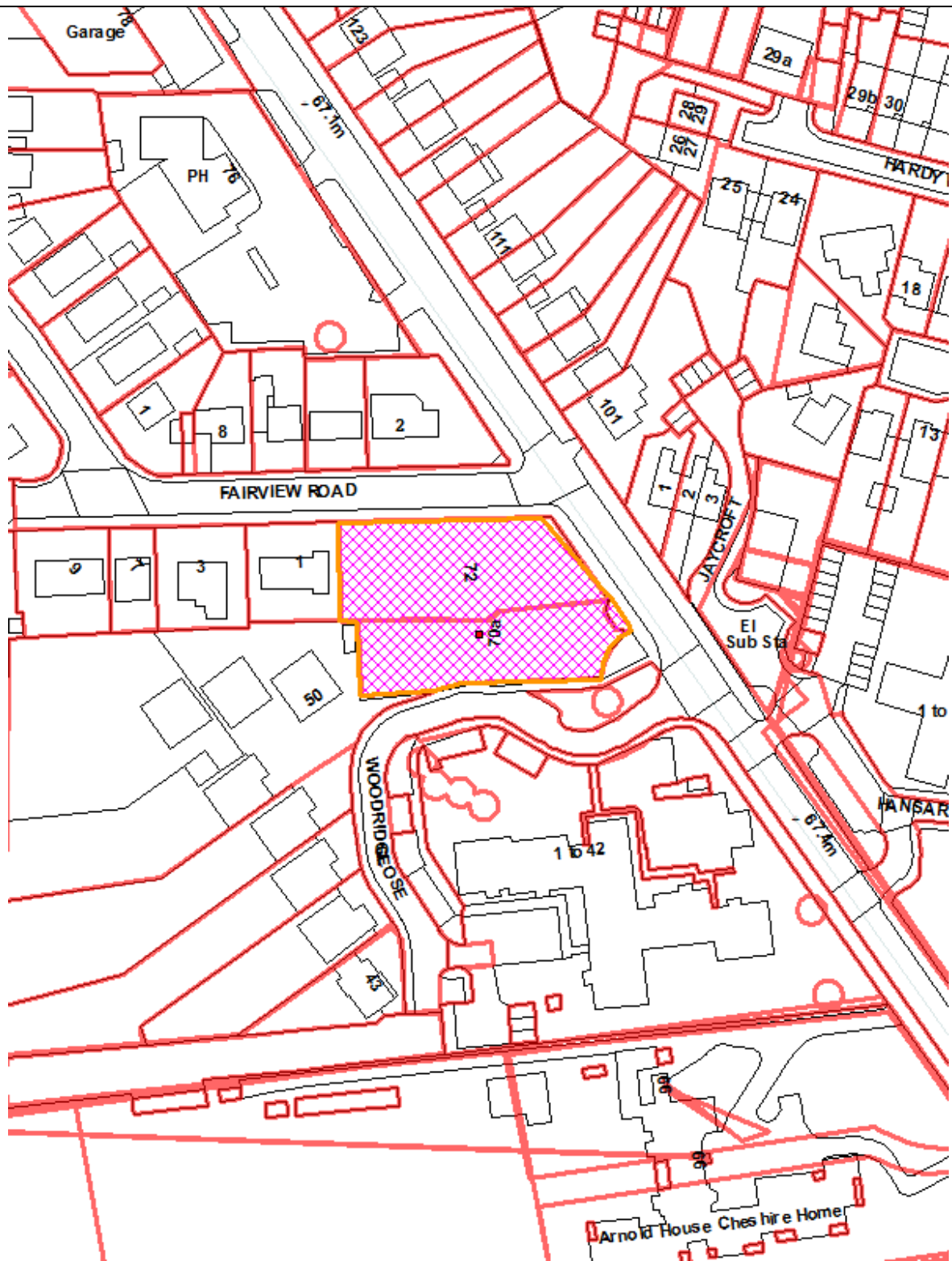
Mr Alfie Yeatman
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RECOMMENDATION: That subject to the completion of a S106 Agreement to secure the obligations as set out in Section 7.54 of this report, the Head of Development Management /the Planning Decisions Manager be authorised to **GRANT** planning permission subject to the conditions.

Note for Members:

Any other application or issue which, by reason of its scale, impact upon the environment, or the level of public or likely Councillor interest, should, in the opinion of the Assistant Director (Regeneration & Planning), be determined by the Committee.

Ref: 18/01539/FUL LOCATION: 70A And 72 The Ridgeway, Enfield, EN2 8JB,



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Scale 1:1250

North



1.0 Executive Summary

- 1.1 The proposed planning application is referred to Planning committee under the adopted Enfield scheme of delegation (adopted 17th October 2017) as per exceptions to the delegated authority under Appendix 1, part 11 as per below,

“Any other application or issue which, by reason of its scale, impact upon the environment, or the level of public or likely Councillor interest, should, in the opinion of the Assistant Director (Regeneration & Planning), be determined by the Committee”.

- 1.2 The proposed development creates nine (9) residential units (formed of (5 x 2 bed, 4 x 3 bed) representing 1250m² of new habitable floorspace meeting the threshold of a major application and under the legislative criteria in the DMPO published in 2015 the Major development and shall be considered at Planning committee.
- 1.3 The proposed development is subject to a s106 legal agreement pertaining to the provision of off-site affordable housing financial contributions (please see section 7.54 of the report). The development shall be subject to planning conditions both pre-commencement and pre-occupation and is considered policy compliant and is recommended for planning approval subject to a s106 legal agreement and planning conditions.

2.0 Site and Surroundings

- 2.1 The site is located on the eastern side of The Ridgeway approximately mid-way along the road. The southern part of the Ridgeway feeds in the developed area of Enfield and is characterised by a mix of detached, semi-detached dwellings and flatted developments. The Ridgeway is made up of a variety of architectural designs. The application site itself is defined by two large detached dwellings, located on generous plots of land. The Ridgeway is classified as an ‘A’ road connecting Enfield with the M25. 70A The Ridgeway benefits from an access into the site off The Ridgeway, where as No 72 The Ridgeway benefits from an access off of Fairview Road which leads off The Ridgeway.
- 2.2 The site once benefited from a tree covered by a Tree Preservation Order (TPO). This tree was felled in 2011, with the agreement of the Tree Officer at the Local

Planning Authority (LPA). There are now no trees on the site covered by a TPO. The site area equates to 00.19 hectares.

- 2.3 The site is not within a Conservation Area nor are the two buildings defined as Listed Buildings. The site has a PTAL Level of 1b representing very poor access to public Transport. Permit parking is in operation on the adjacent road Fairview and The Ridgeway itself has double yellow lines preventing parking.

3.0 Proposal

- 3.1 The proposal seeks planning permission to demolish the two existing two storey detached dwellinghouses on the site and erection of a two storey building with habitable floorspace in the roof. The proposed new building would have a footprint of approximately 502m² forming an “L” shape with the base of the “L” projecting along the southern boundary forward of the principal elevation towards The Ridgeway. The proposal would incorporate eleven (11) gable fronted roof dormers and five rooflights spread across the sloping roof scape and a further ten (10) rooflights on a small crown roof.
- 3.2 The redevelopment of the site would be formed of Nine (9) self-contained flats (5 x 2 bed, 4 x 3 bed). The existing crossover from Fairview Road would be retained and an additional crossover would be created further along Fairview Road. The existing entrance to The Ridgeway would be closed and the provision of twelve (12) formalised parking spaces would be created along the western boundary with The Ridgeway. The site would be landscaped with a number of trees retained and areas sectioned off to create gardens assigned to certain ground floor units. Refuse and secure cycle storage facilities would be located at the rear of the site accessed via the new crossover to the site from Fairview Road.
- 3.3 Changes to the original scheme included:
- Landscape changes to a segregate and assign private amenity space to ground floor units
 - Reduction in the parking provision from 18 spaces to 12 spaces
 - Improved communal amenity space to the rear
 - Re-positioning of ground floor windows
 - Re-location of cycle storage
- 3.4 Further information is provided latter in the report pertaining to the amendments agreed on site.

4.0 Relevant Planning History

Application site

- 4.1 Reference - 17/01298/FUL
Development description - Demolition of existing buildings and redevelopment to provide a total of 9 self-contained flats (6 x 2-bed, 3 x 3-bed) within a 2-storey building including accommodation in roof space, with existing and new vehicular access, car parking and landscaping.
Decision Level – Delegated. Refused
Decision Date – 19/05/2017
- 4.2 Reference - 16/01782/FUL
Development description - Redevelopment of site and erection of a 2-storey block of flats with rooms in roof space comprising 6 x 2 bed, 3 x 3 bed involving front, side and rear dormers, private terraces, new access from Fairview Road and car parking.
Decision Level – Delegated. Refused and APPEALLED
Decision Date – 26/07/2016
Appeal Status – Appeal dismissed under reference: APP/Q5300/W/16/3163001 (Dated 21/07/2018)
- 4.3 Reference - 15/04333/FUL
Development Description - Redevelopment of site and erection of a 2-storey block of flats comprising 4 x 2 bed, 4 x 3 bed involving front and rear dormers, private terraces and patio areas to front and rear, new access from Fairview Road and car parking.
Decision Level – Delegated. Refused and APPEALLED
Decision Date – 04/12/2015
Appeal Status – Appeal dismissed under reference: APP/Q5300/W/16/3147619
- 4.4 Reference - TP/89/1202
Development description - Erection of 2m high boundary wall to garden of existing dwelling abutting Woodridge Close.
Decision level – Delegated
Decision date – 23/11/1989

Enforcement History

- 4.5 No enforcement history exists on site

5.0 Consultation

- 5.1 *Neighbours:* 77 surrounding properties (21 days expired 22nd May 2018) were notified by letter on the 1st May 2018. Following revisions to the proposed landscape layout and floor plans, neighbours were re-notified by letter on the 8th of October. At the time of writing the report, three objects were received by the Council which are summarised below,

- Close to adjoining properties
- Development too high
- Inadequate parking provision
- Information missing from plans
- Loss of Parking
- Loss of privacy
- More open space needed on development
- Noise nuisance
- Not enough info given on application
- Strain on existing services
- Contradictory Reports

Officer Comments

- 5.2 The supporting documents were produced prior to the revised landscaping and ground floor plans, therefore some of the assessment is not succinct, nevertheless the documents remain pertinent. The concerns raised by neighbouring properties shall be covered in the report however the principle concerns are related to parking and privacy issues.

- 5.3 Statutory and Non-Statutory Consultees:

Internal Consultations:

- 5.3.1 Transportation & Transport – The provision of parking spaces and secure cycle storage is acceptable (refer to the transport section for further detailed information). Transport and highways conditions shall be applied. The client will be required to enter in to a s278 agreed as part of the s106 legal agreement to secure the works to the access to the site.
- 5.3.2 Urban design – The siting and massing proposed for the new development is an improvement upon the previous refusals. The massing is suitably set back from the highway. The provision of parking on the frontage has been partially screened by existing natural vegetation and therefore mitigates the impact on the streetscene.
- 5.3.3 SUDs – The inclusion of water gardens on the site is now considered to provide an acceptable level of drainage on the site (please see the drainage section for further information). Conditions shall be applied to the scheme to clarify certain elements of the development.
- 5.3.4 Tree officer – No objections to the loss and re-provision of trees on site subject to a robust landscape condition.
- 5.3.7 Environmental Health – No Objection to the development, pre-commencement planning conditions to be applied to the site to prevent harm.

External Consultations:

- 5.3.8 Thames Water – No response

Officer comments

The consultation responses have directed and facilitated the changes to the development and applicable conditions have been added to secure policy compliant development.

6.0 Relevant Planning Policies

6.1 London Plan (2016)

- 3.3 Increasing housing supply

- 3.4 Optimising Housing potential
- 3.5 Quality and design of housing developments
- 3.9 Mixed and Balanced Communities
- 3.11 Affordable housing targets
- 3.12 Negotiating affordable housing on individual private residential and mixed use schemes
- 3.14 Existing Housing Stock
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.13 Sustainable Drainage
- 5.14 Water Quality and Wastewater Infrastructure
- 5.15 Water Use and Supplies
- 5.16 Waste Self Sufficiency
- 6.9 Cycling
- 6.10 Walking
- 6.13 Parking
- 7.1 Lifetime Neighbourhoods
- 7.3 Designing out Crime
- 7.4 Local Character
- 7.6 Architecture
- 7.19 Biodiversity and access to nature
- 7.21 Trees and Woodland
- 8.2 Planning Obligations
- 8.3 Community Infrastructure Levy

6.2 Core Strategy (2010)

- CP2: Housing supply and locations for new homes
- CP3: Affordable housing
- CP4: Housing quality
- CP5: Housing types
- CP20: Sustainable energy use and energy infrastructure
- CP21: Delivering sustainable water supply, drainage and sewerage infrastructure
- CP22: Delivering sustainable waste management
- CP25: Pedestrians and cyclists
- CP30: Maintaining and improving the quality of the built and open environment
- CP32: Pollution

CP46: Infrastructure contributions

6.3 Development Management Document (2014)

DMD2: Affordable Housing for Development of Less than 10 units
DMD3: Providing a Mix of Different Sized Homes
DMD5: Residential Conversions
DMD6: Residential Character
DMD7: Development of Garden Land
DMD8: General Standards for New Residential Development
DMD9: Amenity Space
DMD10: Distancing
DMD11: Rear Extensions
DMD14: Side Extensions
DMD37: Achieving High Quality and Design-Led Development
DMD45: Parking Standards
DMD46: Vehicle Crossovers and Dropped Kerbs
DMD49: Sustainable Design and Construction Statements
DMD51: Energy Efficiency Standards
DMD58: Water Efficiency
DMD61: Managing Surface Water
DMD68: Noise
DMD81: Landscaping
DMD Appendix 7 - London Plan parking and Cycle standards
DMD Appendix 8 - Parking standards (parking dimensions)
DMD Appendix 9 - Road classifications

6.4 Other Policy

National Planning Policy Framework (2019)
National Planning Practice Guidance
Mayor of London Housing SPG (March 2016)
LBE S106 SPD (November 2016)
Enfield Strategic Housing Market Assessment Update (2015)
Community Infrastructure Levy Regulations 2010

7.0 Analysis

- 7.1 This report sets out the analysis of the issues that arise from the proposal assessed against National, Regional and adopted strategic and local planning policies. The originally submitted plans have been amended and modified as per below,

Amendments to original plans

- 7.2 Following discussions between the applicant and the Council the following amendments have been submitted and considered within the scope of the originally submitted planning application and the Council has re-notified neighbouring properties for further comment.

- On-site car parking spaces have been reduced from 18 to 12 spaces. The parking is now located solely on the frontage of the site
- Formalised covered, secure, lockable and accessible cycle storage has been located to the rear of the site
- The garden area within the site has been separated to provide private amenity space for the ground floor units and a formalised space has been located to the rear of the site
- Modest internal layout and external changes have been undertaken to Flat 1 on the ground floor.

The proposed changes are considered to improve the appearance and quality of the accommodation.

- 7.3 The main issues are considered as follows:

- Background history on site
- Principle of development
- Density of Development
- Design and appearance
- Dwelling Mix
- Standard of accommodation
- Impact on neighbouring amenity
- Traffic and transport implications
- Sustainable design and construction
- Viability - Affordable housing provision

Background history on site:

7.4 The previous planning application (regd no 17/01298/FUL) for a similar form of development (*Demolition of existing buildings and redevelopment to provide a total of 9 self-contained flats (6 x 2-bed, 3 x 3-bed) within a 2-storey building including accommodation in roof space, with existing and new vehicular access, car parking and landscaping*) refused the application on the following four (4) grounds:

- The proposed redevelopment of the site by virtue of the proposed siting, degree of site coverage, proximity to boundaries, its excessive bulk and massing, its relationship to the prevailing form of development in the surrounding area and the expanse of car parking exposed from Fairview Road and The Ridgeway would represent an overdevelopment of the site and result in the creation of an overly dominant, cramped, obtrusive, incongruous and discordant form of development that is out of character and keeping with the surrounding pattern of development. This concern is exacerbated by the forward siting of the building resulting in the loss of established planting on the street frontage and the inability to be able to compensate for that loss. This is contrary to Policies CP5 and CP30 of the Core Strategy, Policies DMD6, DMD8, DMD10, DMD37 & DMD38 of the Development Management Document, London Plan Policies 3.4, 7.4 & 7.6 and the NPPF.
- The proposed design flattened scheme in terms of its elevations and roofscape, by virtue of its insensitive design, its poorly designed roof scape, bulk, excessive size, mass and scale, would result in a bulky, overly dominant, obtrusive and overbearing cramped form of development, resulting in harm to the character and appearance of the visual catchment area within a prominent location in the street scene. The proposal would be contrary to the NPPF (2012), policies 7.4 and 7.6 of the London Plan, CP30 of the Core Strategy as well as Policies DMD 8 and 37 of the Development Management Document and the NPPF.
- The application fails to provide a mechanism for securing contributions towards affordable housing and associated monitoring fees. The proposal is therefore contrary to Policies 3.10, 3.11, 3.12 and 3.13 of the London Plan, Core Policies 3 and 8 of the Enfield Core Strategy, Policy DMD2 of the Development Management Document, the associated S106 Supplementary Planning Document, NPPF and NPPG.

- The proposed development as a result of the poor levels of outlook to flat 1 and flat 3, due to the closeness of the flanking boundary line with Woodridge Close and the proximity to the car parking area respectively would result in substandard from of living accommodation. This is considered to be contrary to policies DMD 8 of the Development Management Document, Policies CP4 and CP30 of the Core Strategy and Policy 3.5 of the London Plan as well as guidance outlined in the London Housing SPG.

7.5 The site has previously been refused and dismissed at Appeal on two occasions. The current proposed scheme differs significantly from the previous schemes and the reasons for refusal have been addressed. The previously refused (Ref 17/01298/FUL) site plan is illustrated below,



Principle of Development

7.6 The proposed development of the site would result in the demolition of two large detached dwellinghouses, No 72 and 70a, both set back from The Ridgeway and both facing the highway with access to No 70a via a crossover off The Ridgeway and No 72 Accessed from Fairview Road. The loss of the two dwelling houses would be replaced with nine (9) flats formed of 6 x 2-bed, 3 x 3-bed, equating to a 66% to 33% split. The principle of new residential development on the site is acceptable meeting the strategic housing needs of Greater London and increasing the housing stock of the Borough in accordance with the National

Planning Policy Framework (NPPF) and the Policy CP5 of the Enfield Core Strategy (2010). However, the development must also be judged on its own merits and assessed in relation to material considerations including the impact on the character of the area and the attainment of appropriate scale, design, amenity space, parking provision, residential amenity and privacy, to achieve a development that integrates appropriately into their surroundings.

- 7.7 The loss of two large dwelling houses while not technically considered a conversion due to the significant and comprehensive scope of demolition on the site, consideration and assessment against Policy DMD 5 (residential conversions) is pertinent. The Ridgeway has witnessed significant historic development and the progressive loss of larger dwellings in favour of flatted redevelopment or conversions. The immediate location is typified by dwellinghouses on the opposite side of the road leading northward and to the west, the majority of which are formed of flatted developments. In this instance the redevelopment and conversion of the site to a flatted development is considered acceptable in principle. The character of The Ridgeway would be able to sustain such a form of redevelopment and it is noted that the previous refusals on the site did not object to the principle of redevelopment for flats.

Dwelling Mix

- 7.8 Policy 3.8 of the London Plan 2016 and Policy CP5 of the Core Strategy (2010) seeks to ensure that new developments offer a range of housing sizes to meet housing needs. The proposal would be in accordance with these policies in addition to Policy 3.3 of the London Plan and Policy CP2 of the Core Strategy, insofar as it would maintain the Borough's housing stock.
- 7.9 The proposed scheme moves towards the requirements of Policy CP5 of the Core Strategy 2010 that expects major developments to provide family sized units in line with the needs identified by the SHMA (Strategic Housing Market Assessment). In this instance the high quality of residential accommodation including residential units in many cases 40% above the minimum floorspace requirements and good communal amenity space. In this context, the proposed

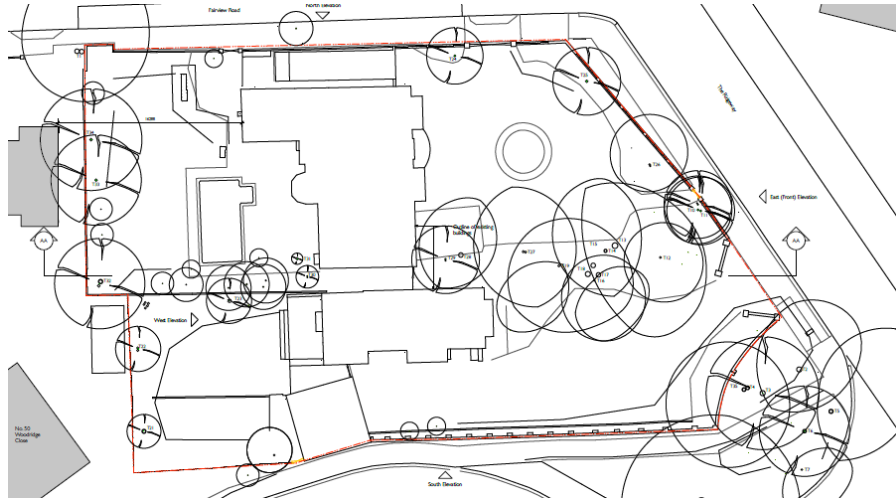
mix is considered to be acceptable and results in a net uplift of three bedroom units above what is currently on site.

Impact on Appearance & Character of the Area

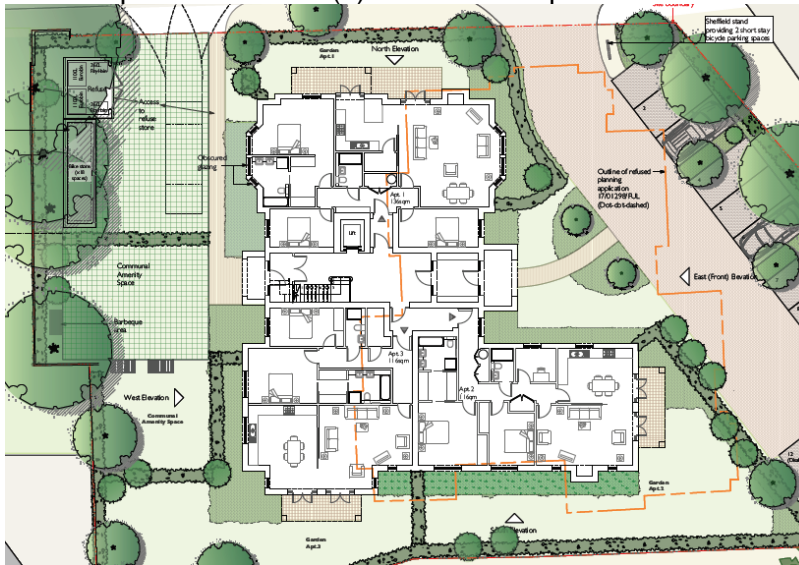
- 7.10 The building would largely respect the existing footprint and siting of the existing two dwellinghouses on site, albeit, projecting to a greater extent to the rear of the site. The proposed “L” shape would include a projecting wing on its southern elevation facing towards The Ridgeway forming the base of the “L” shape. The building would have a crown roof and include eleven (11) pitch roof dormers and fifteen (15) roof lights, ten (10) of which are located on the crown roof element. The dormers are appropriately located on the roof slope and do not appear excessive within the roof form.
- 7.11 The proposed elevations would have a mix of bay window and balcony features complementing the roof forms and reflecting the general character of features on other properties along The Ridgeway. The proposed fenestration is symmetrical along the elevations creating a balanced appearance with a mix of 2/3 panel window formations and larger 2/4 and 2/5 windows and doors.
- 7.12 Parking would be located on the front of the site adjacent The Ridgeway partially screened by trees and landscaping. Twelve (12) car parking spaces would be provided on the frontage accessed by the retained crossover from Fairview Road. The existing crossover access to assigned to No 70a The Ridgeway shall be removed and a strong boundary frontage created. Parking on the frontage of sites along “The Ridgeline” is a prevailing character feature and evident on surrounding sites including the application site. While the use of prominent land for parking on sites should be avoided the use of landscaping (to be conditioned) and existing pattern of parking at the front is considered acceptable within the site context.
- 7.13 A new crossover is proposed to the rear of the site to permit access for refuse trucks and to service the cycle parking provision. The rear of the site shall be landscaped and segregated to create private garden and a communal garden element. The siting of the proposed building permits sufficient space around the perimeter of the building preventing the appearance of excessive development. location shall be

7.14 The design, scale, landscaping and sitting of the building would not be out keeping within the location. The scale of development would not result in an unreasonable intensification of development on the site and considered to optimise the site to its greatest extent without detrimentally overdeveloping the built form or harming the surrounding character.

Existing footprint of No 72/70a



Proposed Footprint of the nine (9) flatted development



Standard of Accommodation

- 7.15 Policy 3.5 of the London Plan 2016 and Policies DMD 5 and DMD 8 of the Enfield Development Management Document (2014) set minimum internal space standards for residential development. The Nationally Described Internal Space Standard applies to all residential developments within the Borough and the London Plan Housing SPG adopted in 2016 has been updated to reflect the Nationally Described Space Standards.
- 7.16 In partnership with the minimum floorspace requirements, new development is expected to provide well-designed, flexible and functional layouts with adequately sized rooms, 2.5m floor-to-ceiling heights, and 20% glazing to all habitable rooms in accordance with the Mayor's Supplementary Housing Guidance.

Table of individual flats proposed floorspace

Flat No	Bed/person	Required Floorspace (m ²)	Provided Floorspace (m ²)
Flat 1	3 Bed/5 Person	86m ²	136m ²
Flat 2	2 Bed/4 Person	70m ²	116m ²
Flat 3	2 Bed/4 Person	70m ²	116m ²
Flat 4	2 Bed/3 Person	61m ²	76m ²
Flat 5	2 Bed/4 Person	70m ²	90m ²
Flat 6	3 Bed/5 Person	86m ²	113m ²
Flat 7	2 Bed/4 Person	70m ²	100m ²
Flat 8	3 Bed/6 Person	95m ²	136m ²
Flat 9	3 Bed/6person	95m ²	144m ²

7.17 The internal floorspace of each dwelling unit would considerably exceed the minimum National internal floorspace standards and all habitable rooms within the proposed flats would have adequate outlook, provision of natural light and good levels of privacy.

7.18 Each proposed unit would have private amenity space and access to communal space at ground floor. Policy DMD 9 (Amenity space) provides the Council's external amenity space standards. The standards below are for dwelling units with access to communal amenity space.

Flat No	Bed/person	Required Floorspace (m ²)	Provided Floorspace (m ²)
Flat 1	3 Bed/5 Person	8m ²	40m ² (Approx)
Flat 2	2 Bed/4 Person	7m ²	50m ² (Approx)
Flat 3	2 Bed/4 Person	7m ²	50m ² (Approx)
Flat 4	2 Bed/3 Person	6m ²	8m ²
Flat 5	2 Bed/4 Person	7m ²	8m ²
Flat 6	3 Bed/5 Person	8m ²	8m ²
Flat 7	2 Bed/4 Person	7m ²	8m ²
Flat 8	3 Bed/6 Person	9m ²	9m ²
Flat 9	3 Bed/6person	9m ²	9m ²

- 7.19 Considering each flat shall have access to private amenity space and approximately 130m² of landscaped external communal amenity space (located at the rear of the site), the Council is satisfied the provision of amenity space complies with Policy DMD 9. The overall quality of accommodation within the nine (9) units is considered acceptable and complies with Policy 3.5 of the London Plan and Policies DMD 8 and DMD 9 of the Development Management Plan.

Impact on neighbouring amenity

- 7.20 Policy 7.6 of the London Plan states that developments should have appropriate regard to their surroundings, and that they improve the environment in terms of residential amenity. Policy CP30 of the Enfield Core Strategy seeks to ensure that new developments are high quality and design-led, having regards to their context. They should help to deliver Core Strategy policy CP9 in supporting community cohesion by promoting attractive, safe, accessible and inclusive neighbourhoods. Policy DMD8 states that new developments should preserve amenity in terms of daylight, sunlight, outlook, privacy, overlooking, noise and disturbance.
- 7.21 The proposed nine (9) flatted residential unit development would result in the footprint of the building projecting to the rear thereby resulting in a closer proximity to No 50 Woodridge Close and No 1 Fairview Road. The proposed site is set back from The Ridgeway and the north facing flank would be of similar distance to No 2 Fairview Road on the opposite side of street to the current location of No 72 The Ridgeway.

Impact on No 1 Fairview Road

- 7.22 No 1 Fairview is located directly to the west of the development site and is orientated to a right angle to the development site resulting in its flank elevation (eastward) facing the rear elevation of the development. The Council acknowledges a first floor side window is located on the flank elevation of No 1 Fairview facing towards the rear elevation of the proposed development. Nevertheless, the window in question is a secondary window and there is a separation distance of approximately 15m between the rear windows of the proposed flats and the subject side window in question. Policy DMD 10 (Distancing) provides the Council's distance approaches between residential units. It states a minimum distance between windows and side boundaries should

be 11m, therefore the proposed relationship between the development and No 1 Fairview would be acceptable.

- 7.23 The proposed footprint and siting of the development would result in the massing of the development projecting further to the rear than the existing footprint of the dwellinghouses No 72 and 70a, however the cumulative overlooking of the rear garden of No 1 Fairview Road would not result in unreasonable harm to existing occupiers of No 1 Fairview Road, and would be in accordance with the distance requirements of policy DMD 10.

Impact on No 50 Woodridge Close

- 7.24 No 50 Woodridge Close is located to the south-west corner of the development site approximately 18m from the proposed south-west corner of the two storey building proposed. The rear elevation of No 50 Woodridge Close is north-east facing however the proposed units on the south-west corner of the development would have windows facing directly west or directly south, therefore the orientation and relationship between the two buildings would result in no direct views between habitable windows.

- 7.25 No balconies or terraces are proposed on the southwest area of the development, further mitigating the potential impact to the privacy levels of occupiers of No 50 Woodridge Close. It is noted No 50 Woodridge Close is a single storey building with habitable floorspace in the roof served by roof lights. The combination of the angle of view from the proposed windows on the flatted development and distance between elevations is adequate to prevent harm to neighbouring amenity in accordance with Policy DMD 8 and DMD 10 of the Development Management Document (2014)

Impact on No 2 Fairview Road

- 7.26 No 2 Fairview Road is located on the north side of Fairview Road on the opposite side of the road to the development site. The proposed footprint of the development would move rearward however the extent of projection would not constitute unreasonable loss of light or result in the loss of privacy to the principal elevational windows on No 2 Fairview Road.

Vehicle Parking & Cycle provision

- 7.27 Policy 6.3 of the London Plan confirms that the impact of development proposals

on transport capacity and the transport network are fully assessed. The proposal must comply with policies cycling (Policy 6.9), walking (Policy 6.10), tackling congestion (Policy 6.11) and parking (Policy 6.13). Policies DMD45 & 47 provide the criteria upon which developments will be assessed with regard to parking standards / layout and access / servicing.

- 7.28 Policy DMD 45 seeks to minimise car parking and to promote sustainable transport options. The Council recognises that a flexible and balanced approach needs to be adopted to prevent excessive car parking provision while at the same time recognising that low on-site provision sometimes increases pressure on existing streets.

Car parking proposals will be considered against the standards set out in the London Plan and:

- a. The scale and nature of the development*
- b. The public transport accessibility (PTAL) of the site;*
- c. Existing parking pressures in the locality;*
- d. Accessibility to local amenities, and the needs of the future occupants of the developments.*

- 7.29 The applicant has provided a Parking Statement, to assess the existing on-street parking in the area and surroundings site of the chase Farm Parking controlled Zone (CPZ) via parking surveys undertaken at various times of the day. The report concludes, *“the results of this assessment show that vehicle parking associated with the proposed residential use can be adequately accommodated on the site and within on-street parking areas with minimal impact to the local streets”*.

- 7.30 The information in the transport statement has been superceded by the changes and alterations on the site, principally the reduction in on-site car parking from 18 spaces to 12 spaces (please see para 4.9).

- 7.31 The site has a PTAL level of 1B considered to represent poor accessibility to public transport. Twelve (12) on-site parking spaces have been provided, two of which meet the dimensions for disabled parking criteria. The proposed breakdown of units to 6 x 2 and 3 x 3 bed units would equate to the requirement for ten and a half (10.5) parking spaces, rounded up to eleven (11) spaces, meeting the London Plan Maximum Parking standards within the parking addendum guidance.

- 7.32 The additional car parking space beyond the eleven (11) required on site would be for visitors and therefore considered acceptable. Fairview Road adjacent the site is part of a CPZ zone and therefore no overflow parking would be available and, should planning permission be granted, a Head of Terms would be added to the s106 legal agreement restricting future occupiers from applying for the Council's on street parking permits. be signed by the applicant. The applicant has provided swept paths to illustrate and prove all spaces would be accessible and useable.
- 7.33 Covered, secure and assessable cycle storage has been located to the rear of the site adjacent the boundary with No 2 Fairview Road. Details of the cycle storage unit have been submitted to the Council and the scale of the storage unit would be capable of accommodating 18 cycles, in addition one Sheffield cycle stand are proposed near the entrance to the site for visitor parking. The cycle provision meets London Plan cycle parking addendum requirements and is therefore acceptable to policy DMD 45.
- 7.34 Policy DMD 46 seeks to ensure that proposals for new vehicular crossovers do not adversely affect traffic flow and road safety, lead to increased pressures on on-street parking or affect the character of the area. No detailed plans have been provided in relation to the proposed new crossover at the rear of the site. Policy DMD 46 (Vehicle crossovers and Dropped kerbs) provides the council's approach to the creation of new crossovers as part of development. The creation of a new crossover in this instance is not considered to have any detrimental impact on the parking capacity in the location. The Council notes that the wider location is characterised by crossovers and therefore an additional crossover would not be out of keeping. No trees would be lost as a result of the proposed crossover and pending a suitable planning condition the crossover is acceptable under the terms of Policy DMD 46.
- 7.35 In line with the London Plan (March 2016), 20% (2.4 spaces) of the total parking spaces should be provided as active electric vehicle (EV) charging points; with a further 20% (2.4 spaces) passive EV charging spaces. This level of provision should be distributed across the whole parking area.

Refuse and Access on site

- 7.36 Secure waste and recycling storage bins are provided externally to the rear of the site. The bin storage is located in close proximity to the proposed new crossover

access on to Fairview Road providing excellent accessibility for refuse operatives. The location of a crossover serving No 2 Fairview Road on the opposite side of the road and the existing width of Fairview Road is sufficient to allow the refuse truck to utilise the proposed crossover and reserve in to the site.

- 7.37 The refuse truck would be able to reserve in to the site on account of the low quantity of traffic on Fairview Road. The proposed layout and management of the site in terms of refuse is considered acceptable, pending a pre-commencement condition providing information regarding the refuse storage units.

Sustainable Drainage

- 7.38 London Plan policies 5.12 and 5.13 require the consideration of the effects of development on flood risk and sustainable drainage respectively. Core Policy 28 (“Managing flood risk through development”) confirms the Council’s approach to flood risk, inclusive of the requirement for SuDS in all developments Policy DMD 61 (Managing Surface Water) expects a Drainage Strategy will be required for all developments to demonstrate how proposed measures manage surface water as close to its source as possible and follow the drainage hierarchy in the London Plan. All developments must maximise the use of and, where possible, retrofit Sustainable Drainage Systems (SuDS) which meet policy requirements.
- 7.39 The applicant has submitted a revised drainage scheme (Prepared EAS, revision Final 4, Dated December 2018) following significant discussions with the Council’s sustainable drainage department. The drainage scheme outlines measure to prevent surface runoff and meet the 1 in 100 year surface water flood risk mitigation measures. Sustainable drainage conditions shall be applied to the site in order to enhance a facilitate the content of the revised drainage scheme document.

Trees & Landscaping

- 7.40 The submitted Arboricultural Method Statement (prepared by David Archer Associates, Dated March 2018) has been reviewed by the Council’s tree officer and considers the removal of the tress to be acceptable and the trees to be removed are of poor quality.

- 7.41 The proposed development site includes extensive landscaping in partnership with the trees and biodiversity report would be conditioned as part of the planning approval. The proposed landscape Plan (Ref 1245-PL018 Rev C, Dated Aug 2018) includes a number of trees that shall be removed as part the development as stipulated in the arboricultural impact assessment (AIA). In order to deal effectively with the loss and retention of trees on the site, a robust landscape plan shall be conditioned on the site to provide high quality replacement trees on the site prior to occupation. The landscape plan shall include but, not be limited to surface materials, plant and vegetation species, soft boundaries, form of enclosure and communal furniture within the site.

Ecology

- 7.42 The applicant has provided a Preliminary Ecology appraisal (dated March 2018, commissioned by David Archer Associates) providing a full assessment of the site and its ecology impacts. There are no perceived ecological constraints preventing the extent of the proposed development, however recommendations are provided in the report post development to encourage bats and roosting birds. A planning condition shall be applied requiring details of ecology improvements to be undertaken.

Energy

- 7.43 Policy 5.2 of the London Plan (2016) expects development proposals to make the fullest contribution to minimising carbon dioxide emission and Enfield Core Strategy Policy CP4 sets a strategic objective to achieve the highest standard of sustainable design and construction throughout the Borough. Policy DMD 50 (Environmental Assessment Methods) required the proposed Development to achieve Code Level 4 (or equivalent rating if this scheme is updated) where it is technically feasible and economically viable to do so. The adopted policies require that new developments achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. A 35% CO2 reduction over Part L of Building Regulations (2013) is required.
- 7.44 The applicant has provided a sustainable design and construction statement providing information on how the development shall meet policy compliant energy efficiently standards. Appendix 1 of the submitted report illustrates the “possible” location of the solar panels on the roof of the new building. The information submitted is helpful however a final energy report would be required prior to

occupation to determine the final location of the solar panels and clarification the building would meet the 35% CO2 reduction over Part L of Building regulations (2013).

Water:

- 7.45 Policy DMD 58 (water Efficiency) expects New residential development, including new build and conversions, will be required to achieve as a minimum water use of under 105 litres per person per day. The applicant has provided no reports or documents to confirm how the proposed development will implement water efficiency measures to achieve usage of less than or equal to 105 litres/person/day for residential developments and incorporate water saving measures and equipment. Therefore a condition shall be applied to the development site.

Section 106 Agreements

Affordable housing contribution

- 7.46 Chapter 5 (Delivering a sufficient supply of homes) of the updated NPPF (January 2019) expects residential developments to provide a size, type and tenure of housing needed for different groups in the community”, forming a core element of housing provision reflected in planning policies”.
- 7.47 Policy 3.13 (Affordable housing Thresholds) of the adopted London Plan 2016 States Boroughs are encouraged to seek a lower threshold through the LDF process where this can be justified in accordance with guidance, including circumstances where this will enable proposals for larger dwellings in terms of floorspace to make an equitable contribution to affordable housing provision.
- 7.48 Following the Court of Appeal decision on 11 May 2016, policies CP3 of the Core Strategy and Policy DMD 2 of the Development Management Document are now defunct and do not sit within the scope of the National Policy exemptions. As per the London plan policy 3.13 and guidance in the DMPO (2015) which has yet to be formally revised the development site is considered to represent a major development site on account of the 1207m² of new residential floorspace. Policy CP3 of the Enfield Core Strategy 2010 seeks 20% off site affordable provision for housing schemes that provide less than 10 dwellings. This position which is

supported by the affordable housing formula within the Enfield S106 Supplementary Planning Document which states also requires 20% financial contribution on sites that propose the development of 1-10 units which have a combined gross floorspace greater than 1,000 square metres.

- 7.49 The applicant has provided a viability report (prepared by Arebray Development consultancy dated March 2019) to justify the lack of any off-site affordable housing contribution. The viability report was independently assessed by a Doug Birt an experienced commercial surveyor specialising in viability of developments. Doug Birt concluded an off-site affordable housing contribution of £161,730.48 (against a normally expected figure of £ 271,296.22 for these development parameters) was possible, alongside a 5% Council management fee of £12,184.52. The applicant has agreed to this figure in writing on the 22/03/2019 and shall now form part of a s106 legal agreement.

CIL Financial Contribution Payable

- 7.50 The development shall pay the following CIL contributions upon commencement of development.

Mayoral CIL

- 7.51 The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought for the scheme is calculated on the net increase of gross internal floor area multiplied by the Outer London weight of £60 together with a monthly indexation figure. It is noted as of the 1st of April 2019 Mayoral CIL has increased to £60/m².

- 7.52 Mayoral community infrastructure levy (CIL) is payable, based on the submitted CIL Form, on the basis of 683 sqm of additional gross floor area net of the existing houses, which from 1 April 2019 will be calculated at £60 per sqm:

$$683 \text{ sqm} \times £60 \times 318 / 223 = £ 58,437.85$$

Enfield CIL

- 7.53 On 1 April 2016, the Council introduced its own CIL. The money collected from the levy (Regulation 123 Infrastructure List) will fund rail and causeway infrastructure for Meridian Water.

- 7.54 The Council CIL payment should therefore be as follows based on the estimated net additional gross floorspace in the submitted CIL form:
683 sqm x £120 per sqm = £ 81,960.00
- 7.55 The proposed off-site affordable housing contribution and CIL contribution on-site are in accordance with NPPF guidance, London Plan policy 3.13 and guidance within the adopted Section 106 SPD guidance adopted in (November 2016).

8.0 Conclusion

- 8.1 The development provides nine (9) high quality residential units formed of 6 x 2 and 3 x 3 with generous internal floorspace provision. The siting and massing of the proposed two storey building in tandem with the orientation of the building in relation to the neighbouring context and the location of windows and balconies would not result in harm to neighbouring amenity levels. The on-site Parking provision meets London Plan criteria and future occupiers shall be restricted from receiving parking permits within the CPZ. The landscaping of the site shall be conditioned to improve the appearance of the site and provide excellent external amenity space to future residents of the site. The off-site affordable housing provision shall be secured via a s106 legal agreement and all other aspects of the development are acceptable and shall be secured via pertinent planning conditions.

9.0 Recommendation

That, **PLANNING PERMISSION BE GRANTED** subject to a s106 legal agreement and planning conditions;

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

2. The development hereby approved shall only be laid out as 9 self-contained units comprising 6 x 2-bed, 3 x 3-bed as shown on the drawings. There shall be no

deviation from the number, size or mix of units from that approved without the prior approval of the Local Planning Authority.

Reason: Having regard to securing an appropriate mix in the number and size of units and having regard to adopted parking standards.

3. No above ground works shall commence until details of the external finishing materials to be used have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

4. No above ground works shall commence until detailed drawings at a scale of 1:20 detailing the proposed architectural features (including quoins, soldier courses, oriel windows, window frames and window reveals) have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

5. No above ground works shall commence until details of the surfacing materials to be used within the development including footpaths, access roads and parking areas and road markings have been submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure a satisfactory visual appearance and in the interests of highway safety

6. No excavation shall commence until details of existing planting to be retained and trees, shrubs and grass to be planted, and the treatment of any hard surfaced amenity areas have been submitted to and approved in writing by the Local Planning Authority. The site shall be landscaped in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any trees or shrubs which die, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: In the interests of visual amenity and to enhance the ecological value of the site in accordance with DMD 79

7. The site shall be enclosed in accordance with details to be submitted to and approved in writing by the Local Planning Authority. The means of enclosure shall be erected in accordance with the approved details before the development is occupied.

Reason: To ensure satisfactory appearance and safeguard the privacy, amenity and safety of adjoining occupiers and the public and in the interests of highway safety.

8. The development, excluding demolition and ground clearance, shall not commence until plans detailing the existing and proposed ground levels including the levels of any proposed buildings, roads and/or hard surfaced areas have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure that levels have regard to the level of surrounding development, gradients and surface water drainage.

9. Prior to the commencement of above ground works, details of the siting and design of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield – Waste and Recycling Planning Storage Guidance ENV 08/162, have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction target.

10. The parking area forming part of the development shall only be used for the parking of private motor vehicles and shall not be used for any other purpose.

Reason: To ensure that the development complies with Development Plan Policies and to prevent the introduction of activity which would be detrimental to amenity.

11. The development shall not commence until a revised Sustainable Drainage Strategy has been submitted to and approved in writing by the Local Planning

Authority. The details shall be based on the disposal of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework and should be in line with our DMD Policy SuDS Requirements:

- a) Shall be designed to a 1 in 1 and 1 in 100 year storm event with the allowance for climate change
- b) Follow the SuDS management train and London Plan Drainage Hierarchy by providing a number of treatment phases corresponding to their pollution potential
- c) Should maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value
- d) The system must be designed to allow for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact
- e) Clear ownership, management and maintenance arrangements must be established
- f) The details submitted shall include levels, sizing, cross sections and specifications for all drainage features

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy

12. Prior to first occupation of the development approved, a verification report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing.

Reason: In the interest of managing surface water runoff as close to the source as possible in accordance with adopted policy.

13. The development shall not commence until a construction management plan has been submitted to and approved by the Local Planning Authority. The construction management plan shall be written in accordance with London Best Practice Guidance and contain:

- a. A photographic condition survey of the public roads, footways and verges leading to the site.
- b. Details of construction access and associated traffic management.
- c. Arrangements for the loading, unloading and turning of delivery, construction and service vehicles.

- d. Arrangements for the parking of contractors' vehicles.
- e. Arrangements for wheel cleaning.
- f. Arrangements for the storage of materials.
- g. Hours of work.
- h. The storage and removal of excavation material.
- i. Measures to reduce danger to cyclists.
- j. Dust mitigation measures.
- k. Membership of the Considerate Contractors Scheme

The development shall be carried out in accordance with the approved construction management plan unless otherwise agreed by the Local Planning Authority.

Reason: To ensure construction does not lead to damage of the nearby public road network and to minimise disruption to the neighbouring properties.

14. The development shall not commence until an undertaking to meet with best practice under the Considerate Constructors Scheme and achieve formal certification has been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not adversely impact on the surrounding area and to minimise disruption to neighbouring properties.

15. No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure and piling has the potential to impact on local underground sewerage utility infrastructure.

16. Prior to any development commencing, inclusive of site clearance, details of a Construction Waste Management Plan shall be submitted to the Local Planning

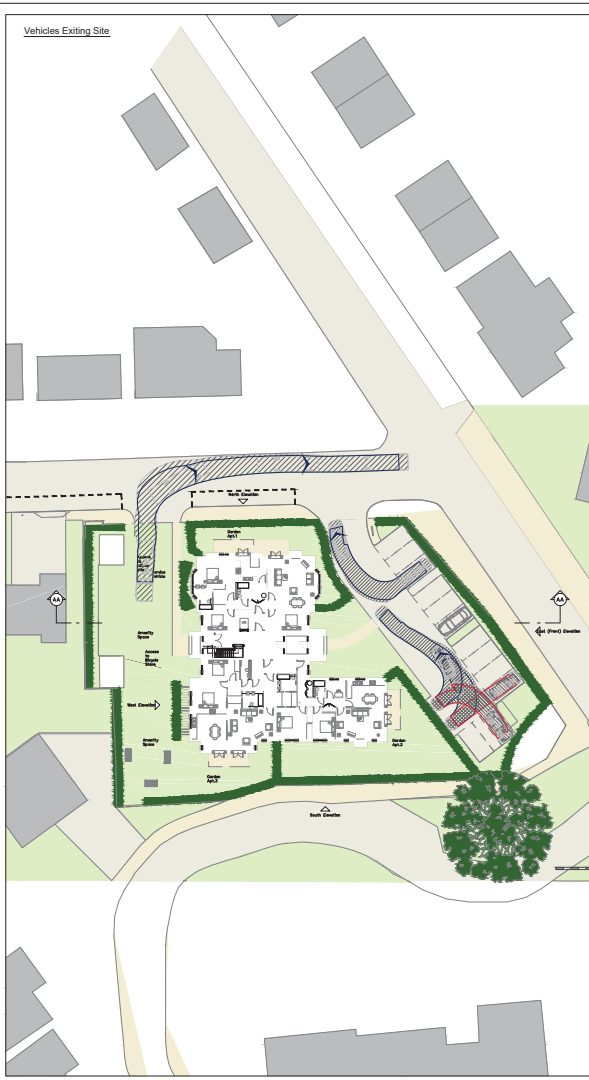
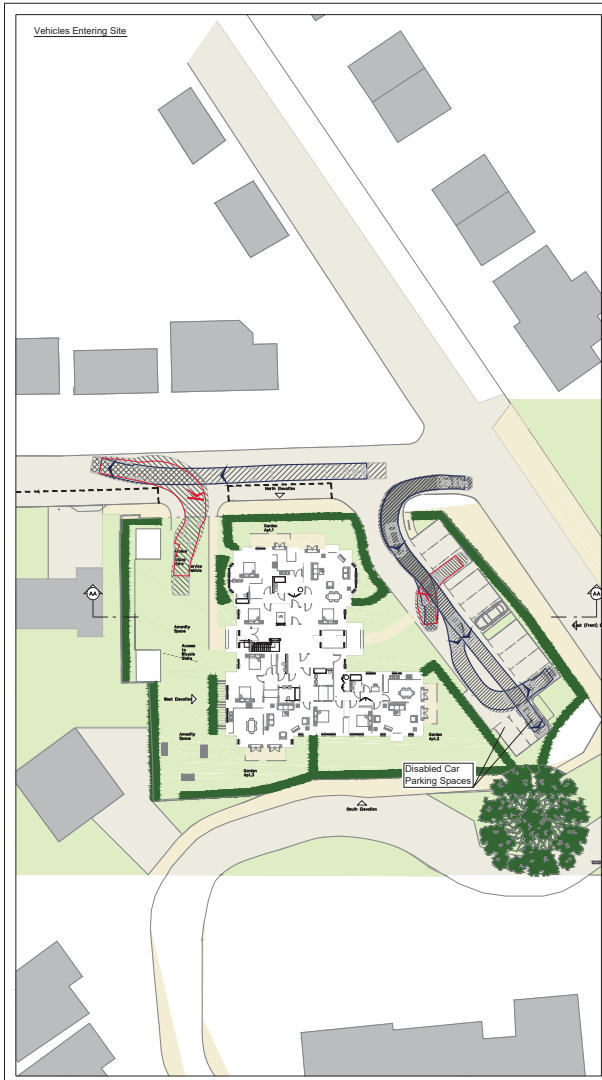
Authority for approval in writing. The Construction Waste Management Plan shall include as a minimum:

- a. Target benchmarks for resource efficiency set in accordance with best practice;
- b. Procedures and commitments to minimize non-hazardous construction waste at design stage. Specify waste minimisation actions relating to at least 3 waste groups and support them by appropriate monitoring of waste;
- c. Procedures for minimising hazardous waste;
- d. Monitoring, measuring and reporting of hazardous and non-hazardous site waste production according to the defined waste groups (according to the waste streams generated by the scope of the works);
- e. Procedures and commitments to sort and divert waste from landfill in accordance with the waste hierarchy (reduce; reuse; recycle; recover) according to the defined waste groups; and
- f. No less than 85% by weight or by volume of non-hazardous construction, excavation and demolition waste generated by the development has been diverted from landfill

Reason: To maximise the amount of waste diverted from landfill consistent with the waste hierarchy and strategic targets set by Policies 5.17, 5.18, 5.19 of the London Plan.

17. Prior to the occupation of the development details for the provision of a communal television systems/satellite dishes have been submitted to and approved in writing by the Local Planning Authority. The development shall only be undertaken in accordance with the approved detail.

Reason: In order to mitigate the possibility of numerous satellite dishes being installed on the buildings hereby approved in the interests of the visual appearance of the development, in particular, and the locality in general.



Notes:
 1. This drawing is based upon drawing number 1245-PL010 - OPT.1 Rev D supplied by Hub Architects and Icen Projects Ltd. shall not be liable for any inaccuracies or deficiencies.

Vehicle Profile:

7.5t Box Van	8.010m
Overall Length	2.100m
Overall Width	3.556m
Overall Body Height	0.351m
Min Body Ground Clearance	2.564m
Track Width	4.00s
Lock-to-lock time	7.400m
Curb to Curb Turning Radius	

Skoda Octavia	4.572m
Overall Length	1.765m
Overall Width	1.488m
Overall Body Height	0.249m
Min Body Ground Clearance	1.713m
Max Track Width	4.50s
Lock-to-lock time	5.100m
Curb to Curb Turning Radius	



A	13/08/18	Revised Tracking	FA	RB	RA
Rev	Date	Amendments	Drawn	Chk	App

Iceni Projects
 Filcroft House
 114-116 Charing Cross Road
 London, WC2H 0JR
 T 020 3640 8508
 F 020 3435 4228
 mail@iceniprojects.com



Client
 Landvest Developments Limited

Project
 70A and 72 The Ridgeway, Enfield

Title
 Swept Path Analysis
 (7.5t Box Van)

Drawn By	Checked By	Approved By
FA	RB	RA

Scale @ A3	Date
1:250	13/08/2018

Project No.	Drawing No.	Rev.
18-T021	07	A

ICENI PROJECTS ACCEPTS NO RESPONSIBILITY FOR ANY UNANTICIPATED CONSEQUENCES TO THIS DRAWING. ANY SIGNED DRAWINGS ARE TO BE MARKED AS SUCH.

bc THE BIKE STORAGE COMPANY

20 Space Amazon Eco Cycle Shelter – Specification Sheet

A beautifully wood clad cycle shelter, ideal for those seeking green credentials. Made from FSC timber this is an extremely environmentally friendly bike shelter. With a wide range of options to adapt this cycle shelter to your environment this proves to be an extremely popular choice for bike storage across a wide range of applications.

- 2,100mm height
- 8,100mm length
- 2,100mm deep
- Hot dipped galvanised box section outer frame with optional powdercoating service
- Available with galvanised mesh/wood clad sliding & swing gates
- Sides & rear fully clad in FSC certified timber
- Hasp & staple security system (swipe card/ Diglock available upon request)




bc THE BIKE STORAGE COMPANY

High Security Mesh System – Specification Sheet

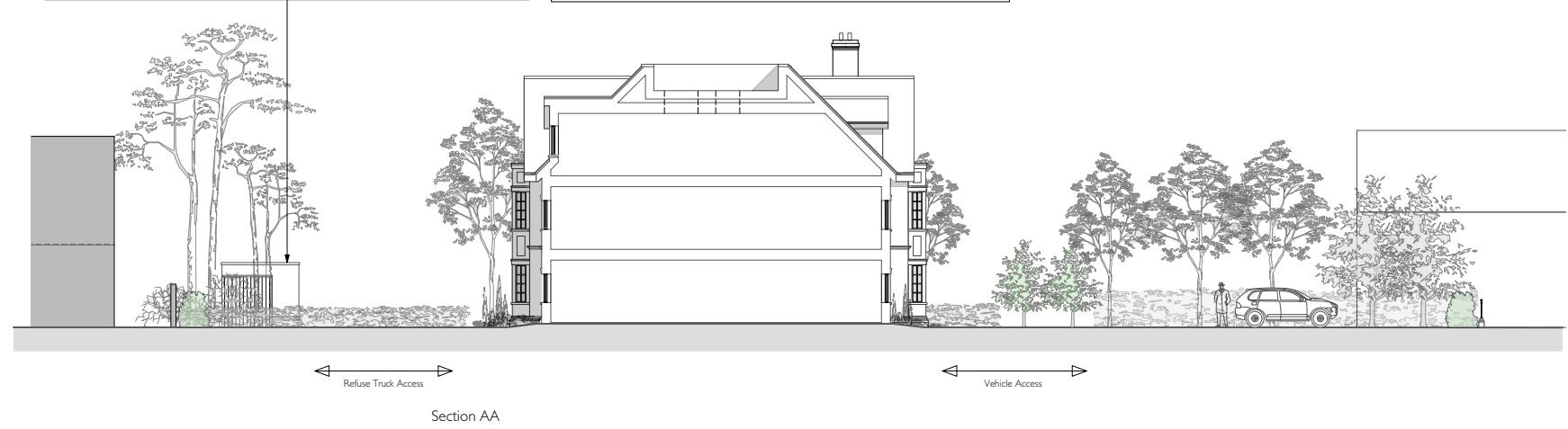
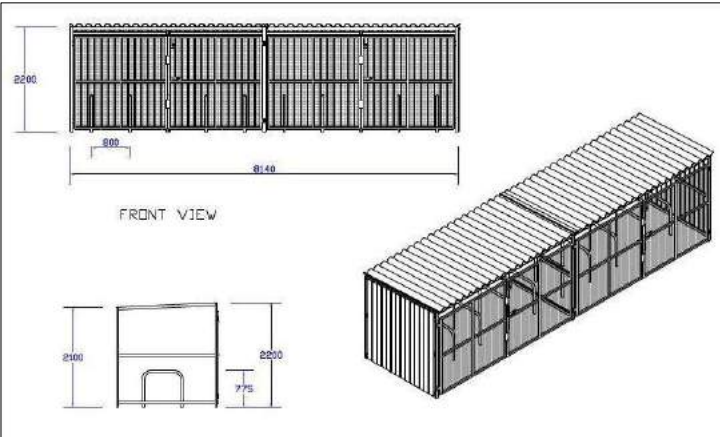
Our High Security Mesh System is designed to offer exceptional bike parking security in areas such as underground car parks, between archways or pre-existing structures.


Customised to fit your exact requirements, cycle parking capacity and security requirements. Perfectly suited to high security applications, providing excellent see through visibility extensively used with CCTV.

Ultra-secure 76.2mm x 12.7mm aperture with 4mm wires, supplied with 80 x 80mm SHS posts.

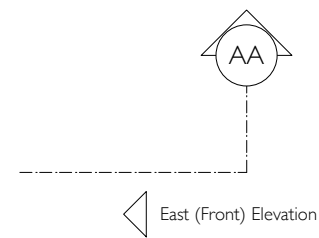
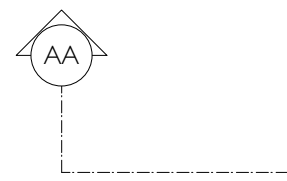
- Available in hot dipped galvanised or polyester powdercoated to any RAL colour
- Unique tamper proof clamp fixing system
- Baseplate or roof fixed
- Secured by Design compliant
- Other colours/finishes available upon request
- Supplied in 1.8m, 2.0m, 2.4m, 2.7m and 3.0m heights





	<p>15 HOOPERS YARD LONDON NW6 7EJ t : 020 7328 2576 f : 020 7624 7811 Email: info@hubarchitects.co.uk</p>	<p>All dimensions are to be checked on site before commencement of works. All sizes and dimensions to any structural elements are indicative only. See structural engineer's drawings for actual sizes/dimensions. Sizes of and dimensions to any service elements are indicative only. See service engineer's drawings for actual sizes/dimensions. This drawing to be read in conjunction with all relevant Architect's drawings, specifications and other consultants' information.</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>Rev.</th> <th>Date</th> <th>Description</th> </tr> <tr> <td>/</td> <td>17/04/18</td> <td>Issued for Planning</td> </tr> <tr> <td>A</td> <td>19/09/18</td> <td>Bike Store amended - Planning officer's comments .</td> </tr> <tr> <td>B</td> <td>28/09/18</td> <td>Bike Store amended - to Planning officer's comments .</td> </tr> </table>	Rev.	Date	Description	/	17/04/18	Issued for Planning	A	19/09/18	Bike Store amended - Planning officer's comments .	B	28/09/18	Bike Store amended - to Planning officer's comments .	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>Initial</th> <th>Rev.</th> <th>Date</th> <th>Description</th> </tr> <tr> <td>PK</td> <td></td> <td></td> <td></td> </tr> <tr> <td>DP</td> <td></td> <td></td> <td></td> </tr> <tr> <td>NAN</td> <td></td> <td></td> <td></td> </tr> </table>	Initial	Rev.	Date	Description	PK				DP				NAN				<h2 style="margin: 0;">PLANNING</h2>
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<p>Project: 70a-72 The Ridgeway, EN2 8JB Drawing: Proposed Section AA</p>		<p>Scale: 1:100@A1, 1:200@A3 DWG: 1245 - PL017</p>		<p>Date: Feb 2018 Revision: B</p>																													

North Elevation

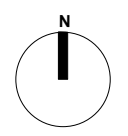



East (Front) Elevation

West Elevation



South Elevation



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<p>Project: 70a-72 The Ridgeway, EN2 8JB Drawing: Proposed First Floor Plan</p>																												
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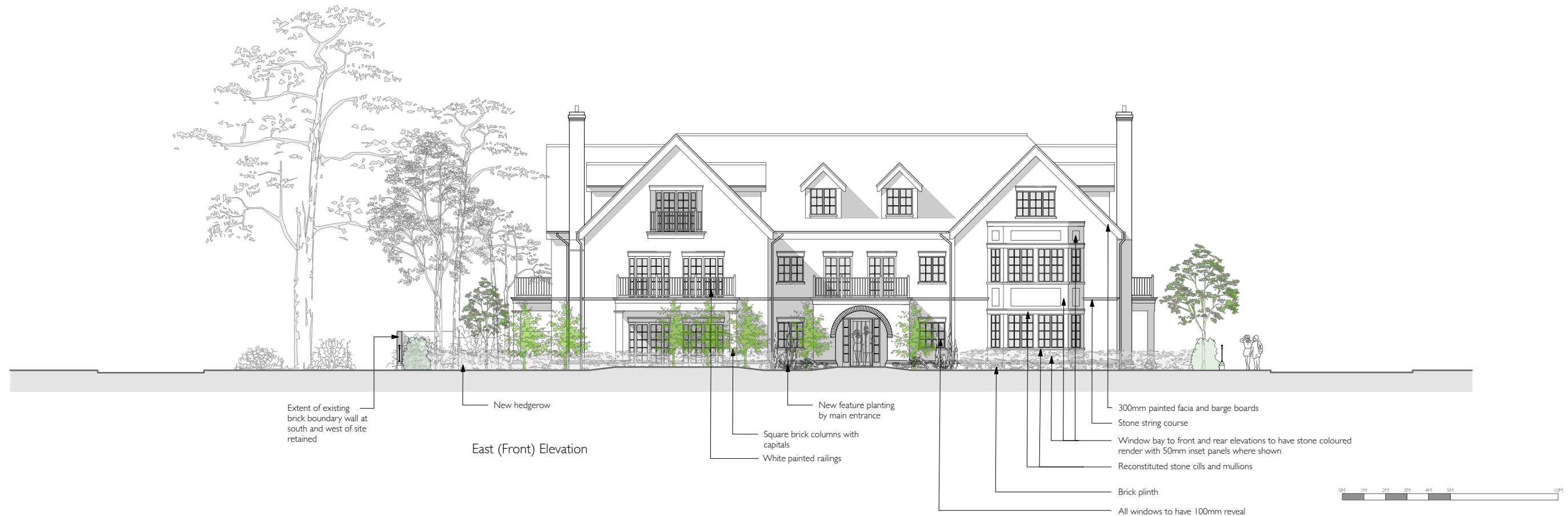


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Rev.	Date.	Description.	Initial.	Rev.	Date.	Description.	Initial.
/	17/04/18	Issued for Planning	PK				
A	28/09/18	Landscape amended to Planning officers comments	NAN				

PLANNING	
Project: 70a-72 The Ridgeway, EN2 8JB	Drawing: Proposed Block plan
Scale: 1:200@A1	DWG: 1245 - PL008
Date: Feb 2018	Revision: A



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Rev.	Date.
/	17/04/18
A	19/09/18

Description.
Issued for Planning
Landscaping revised following comments

Initial.	Rev.	Date.
PK		
DP		

Description.

Initial.

PLANNING

Project:	70a-72 The Ridgeway, EN2 8JB	Drawing:	Proposed North & East Elevations
Scale:	1:100@A1, 1:200@A3	DWG:	1245 - PL014
Date:	Feb 2018	Revision:	A

AREA COMPARISON CHART (msq)

Scheme	Building Footprint	Hardstanding	Amenity Space
Existing	336	793	738
16/01782	580	431	902
17/01298	547	435	911
18/01539	504	606	794
Option I	504	378	974

Amazon Eco Cycle Shelter with sheffield racks by **The Bike Store Company** :

- Clad in FSC timber
- Galvanised mild steel frame
- Security lock - Diglock
- Wood clad sliding gates



CAR PARKING
 20% of car parking spaces to have active EV (Electric Vehicle) charge points
 20% of car parking spaces to have passive provision

No. 50 Woodridge Close



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All dimensions are to be checked on site before commencement of works.
 All sizes and dimensions to any structural elements are indicative only. See structural engineer's drawings for actual sizes/dimensions.
 Sizes of and dimensions to any service elements are indicative only. See service engineer's drawings for actual sizes/dimensions.
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Rev.	Date	Description	Initial
/	17/04/18	Issued for Planning	PK F
A	21/06/18	Apartment 1 - Master Bedroom, dressing area and ensuite layout reconfigured. Car park charge points information added. Two short stay bicycle parking spaces added.	PK G
B	17/07/18	Areas of amenity space increased, number of parking spaces reduced.	PK
C	26/07/18	Areas relating to 16/01782 scheme added.	PK
D	08/08/18	Landscaping amended following site meeting with Enfield Case Officer on 7/8/2018.	PK
E	12/09/18	Landscaping amended following comments from Planning officer.	PK

Rev.	Date	Description	Initial
F	19/09/18	Bike Store amended - Planning officer's comments.	PK
G	28/09/18	Parking Bay amended to Planning officer's comments.	PK

Initial DP
 NAN

PLANNING

Project:	70a-72 The Ridgeway, EN2 8JB	Drawing:	Proposed Ground Floor Plan Option I
Scale:	1:200@A1	DWG:	1245 - PL010 - OPT.1
Date:	Feb 2018	Revision:	G



South Elevation



West (Rear) Elevation



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Rev.	Date.
/	17/04/18

Description.
Issued for Planning

Initial.	Rev.	Date.
PK		

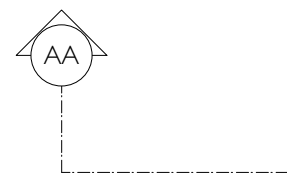
Description.

Initial.

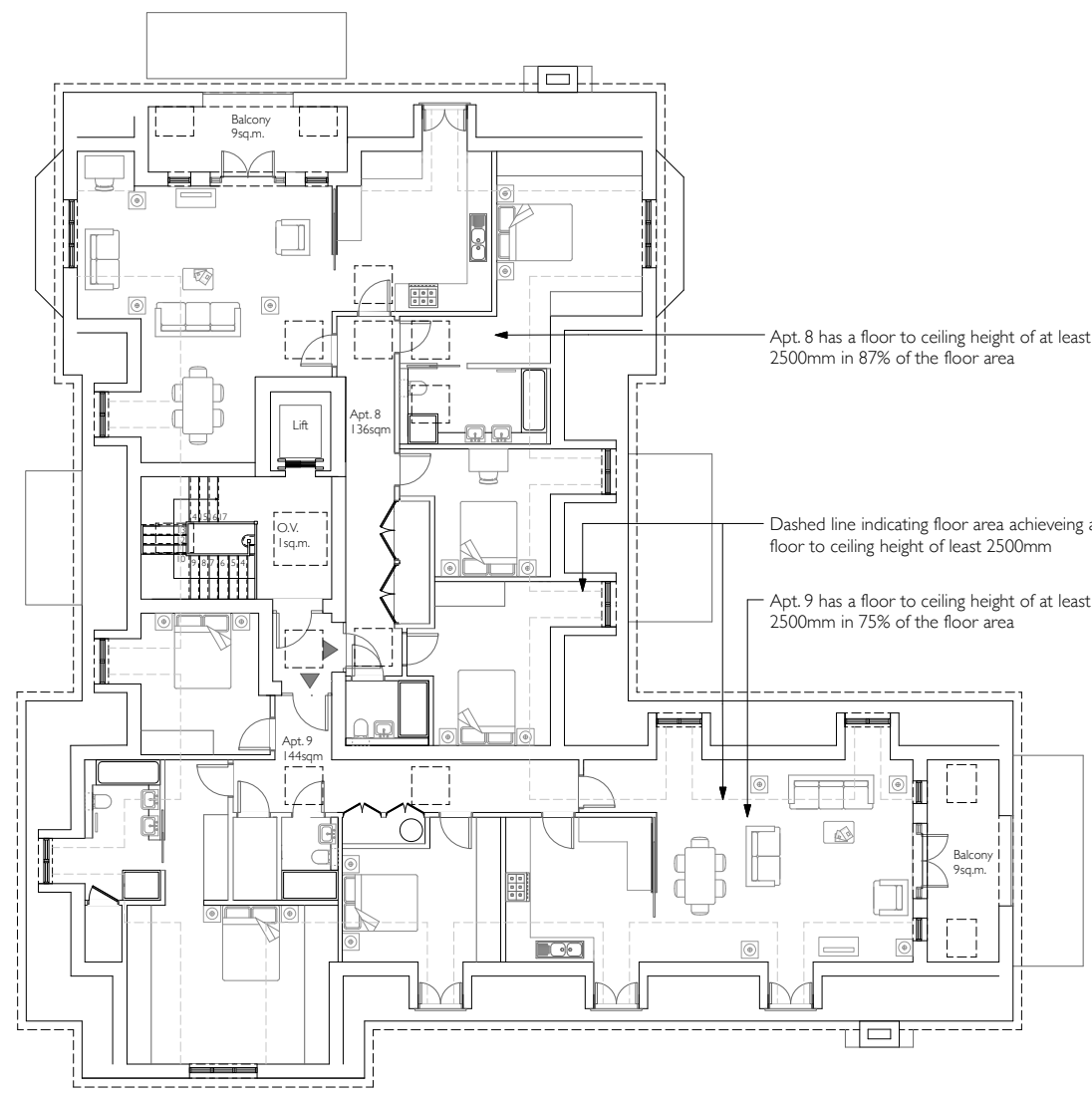
PLANNING

Project:	70a-72 The Ridgeway, EN2 8JB	Drawing:	Proposed South & West Elevations
Scale:	1:100@A1, 1:200@A3	DWG:	1245 - PL015
Date:	Feb 2018	Revision:	/

North Elevation



West Elevation



Apt. 8 has a floor to ceiling height of at least 2500mm in 87% of the floor area

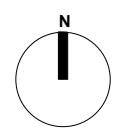
Dashed line indicating floor area achieving a floor to ceiling height of least 2500mm

Apt. 9 has a floor to ceiling height of at least 2500mm in 75% of the floor area



East (Front) Elevation

South Elevation



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Rev.	Date.	Description.	Initial.	Rev.	Date.	Description.	Initial.
/	17/04/18	Issued for Planning	PK				
A	30/05/18	Floor to ceiling height of areas achieving at least 2500mm indicated	PK				
B	08/10/18	Minor amendments.	DP				

PLANNING

Project:	70a-72 The Ridgeway, EN2 8JB	Drawing:	Proposed Second Floor Plan
Scale:	1:100@A1, 1:200@A3	DWG:	1245 - PL012
Date:	Feb 2018	Revision:	B